

CITY OF *stillwater*

Corridor Redevelopment Plan



Adopted June 4, 2012

Stillwater Corridor Redevelopment Plan

INTRODUCTION:

The Corridor Redevelopment Plan for the City of Stillwater is developed to identify and encourage innovation thinking when evaluating development proposals and/or opportunities in the area immediately south and east of the Oklahoma State University campus. Stillwater is the county seat of Payne County and the home to Oklahoma State University. With these come responsibilities to adequately and appropriately plan for the needs of the permanent residents and business owners as well as the needs of the University students. City and University officials recognize that the appearance and commercial strength of the City, and particularly the core commercial areas adjacent to campus and the downtown area, are essential to maintain and provide opportunities to progress and strengthen in the future. A mix of traditional downtown uses is established in the downtown core area and further established for development as the Business Improvement District #1 (BID). The connection between the BID and the University is wrought with scattered highway commercial, core commercial areas, single-family uses, multi-family uses, institutional uses, and no established scheme for transportation or pedestrian options, nor for public spaces and art.

Over the past few years, studies and plans have been prepared – some of which were implemented, some put on the proverbial shelf, and others partially implemented. Revitalization has succeeded in the BID area; storefronts on The Strip and on Campus Corner have maintained occupancy and attractive and inviting appearances. With the potential for a future that involved higher density redevelopment, commercial and residential mixed uses, pedestrian access,



Where Oklahoma Began



Downtown Landscaped Intersection



Downtown at Night

public spaces and public art, and a hard and fast connection from the University to Downtown Stillwater, the City is taking direct action through this plan to encourage redevelopment efforts, new investment, improved infrastructure, highest and best utilization of properties, public spaces for the enjoyment of all, and economic strength and vitality for the surrounding area of the community.

Therefore, the City of Stillwater has created the following Corridor Redevelopment Plan as an effective long-range approach to facilitate development in the area identified herein.



Public Space

Reference Copy

PLAN AREA:

The Corridor Redevelopment Plan area is generally bounded by 6th Avenue (US Highway 51) on the south, Washington Street on the west, Hall of Fame on the north, and Main Street on the east. The area consists of 512 parcels that support a mixture of land use types including commercial, mixed use, institutional, parking, office use, University use, single-family residential use, multi-family residential use, and vacant lands. The area encompasses approximately 120 acres, including public street rights-of-way and alleys. Due to various factors, substantial private and public investment has not occurred in the area.

This plan has been prepared to guide the direction of future developments and redevelopments and to promote a civic and functional relationship between the public streetscape and adjacent private development. The information contained herein provides a clear policy for evaluating future developments with regard to design quality. Through sensitive application of the design principles included, it is intended that each project will succeed in enriching the cultural, livability, and economic vitality of the area.

GOALS AND OBJECTIVES:

The Corridor Redevelopment Plan seeks to respond to a number of issues and market conditions anticipated over the next 20 years. This plan is indicative of a strong commitment on the part of the City of Stillwater and the University to continue the revitalization efforts that have provided capital and redevelopment opportunities in other areas of the community. The primary goal of the Corridor Redevelopment Plan is to strengthen the residential, employment, public, and commercial components of the area through the improvement of existing



Pedestrian Corridor



Typical Downtown Street



Downtown Stillwater

utilities and facilities and to induce the redevelopment of existing deteriorated, underutilized, and vacant buildings and sites for new and/or improved uses. This plan is intended to provide a framework for improvements within the project area over a 20-year period, including redevelopment goals, recommended land uses, financial assistance opportunities, and specific zoning districts or overlays.

These overall goals and objectives are intended to guide decisions as summarized herein:

- ❖ Promote the developed core commercial areas of Stillwater as centers of business activity and economic growth.
- ❖ Encourage private investment to upgrade existing deteriorated buildings and sites.
- ❖ Encourage a diversity of housing types and densities which satisfy a wide-range of needs for all persons.
- ❖ Encourage private investment to redevelop vacant and underutilized buildings and sites.
- ❖ Explore carefully selected and positive financial and regulatory incentives that support the retention, expansion, and relocation of uses to and within this Plan area.
- ❖ Expand and upgrade existing city facilities to provide the necessary infrastructure for redevelopment while encouraging private financial investment.
- ❖ Promote, retain, and attract businesses and investments that provide a diverse base of employment opportunities.
- ❖ Provide the necessary beautification improvements to attract and encourage public and/or private investment.



Typical Building Ornamentation



Pedestrian Corridor



Store Fronts

- ✧ Provide a visible and useable connection between the Oklahoma State University campus and the three primary core commercial districts: The Strip, Campus Corner, Downtown Stillwater.

THE PLAN:

The revitalization of the plan area is a large and complex undertaking that presents many challenges and opportunities. The success of this effort will depend on the cooperation between the private sector, the University and the City of Stillwater. The adoption of this Corridor Redevelopment Plan, along with any incentive program approved by the City Council, will aid in the implementation of the goals and objectives of the City and will assist in stimulating redevelopment and rehabilitation in this area, which may not otherwise be anticipated to occur.

The purpose of this Corridor Redevelopment Plan is to promote the health, safety, economics, opportunities, and welfare of the general public by:

- ✧ Creating an environment conducive to pedestrian and bicycle circulation and providing good connectivity to the core commercial areas, University and nearby residential neighborhoods;
- ✧ Eradicating blighting conditions and instituting conservation and sustainability measures;
- ✧ Removing and alleviating adverse conditions by encouraging private investment of underutilized and vacant properties which will strengthen the economy, tax base, business environment, living environment, pedestrian opportunities, and connections between core commercial areas;
- ✧ Improving existing public utilities within the area; and
- ✧ Enhancing the overall quality of the



Pedestrian Corridor



Pedestrian Corridor



Blighted Conditions

City of Stillwater.

EXISTING LAND USE:

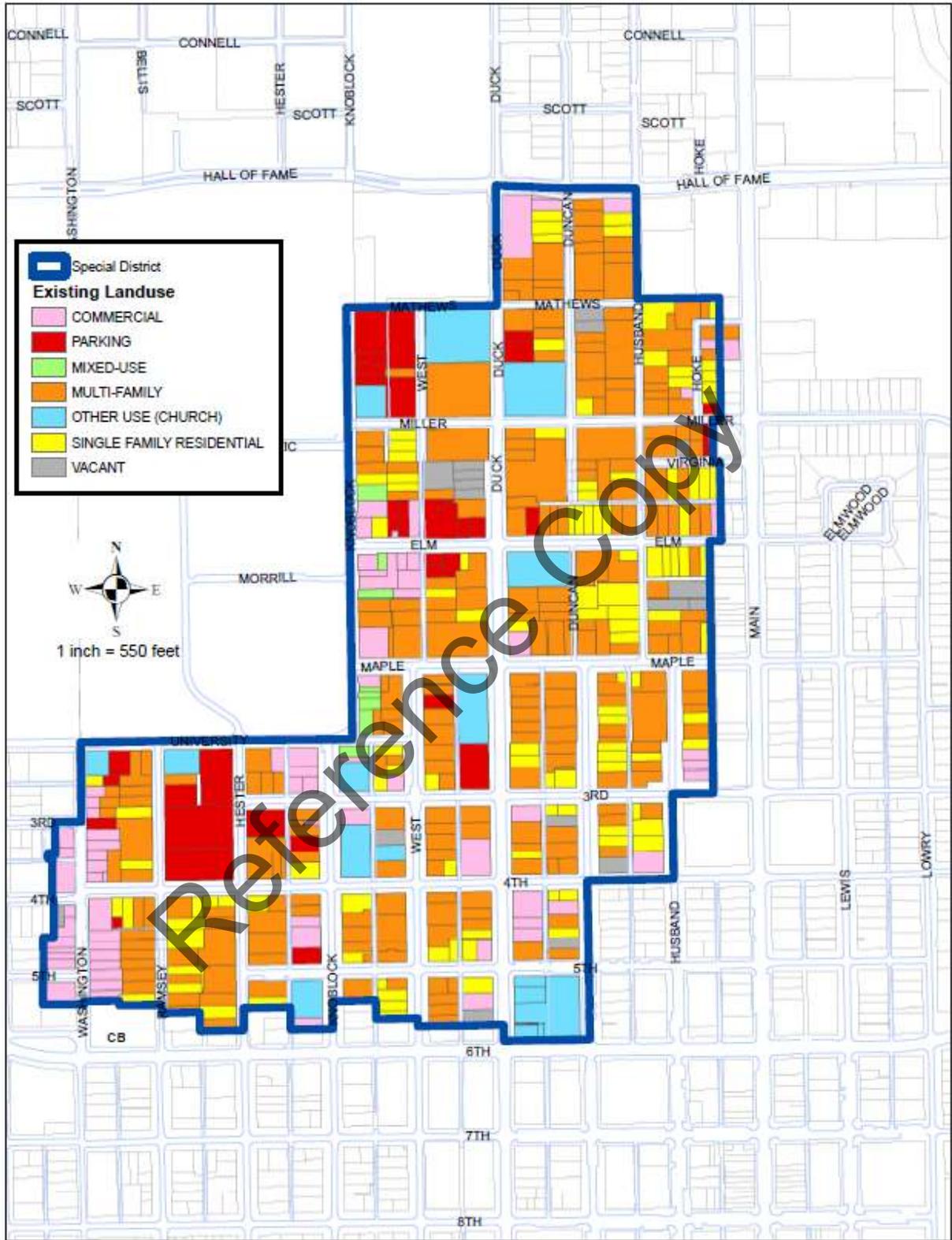
Within the Corridor Redevelopment Plan area, each individual lot or parcel of land has a use associated with it. Over time, such uses change but at the time of this plan, there are certain uses of land that are existing. In order to establish a beginning point from which to encourage any redevelopment, identification of existing uses must be made.

The following map indicates the existing land uses for properties within the plan area. These uses are categorized as single family residential, commercial, parking, mixed-use, multi-family, other use, and vacant.



Corner Store Front

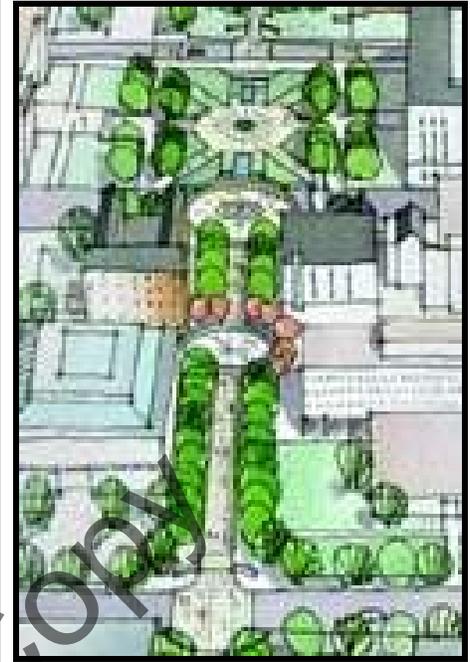
Reference Copy



FUTURE LAND USE:

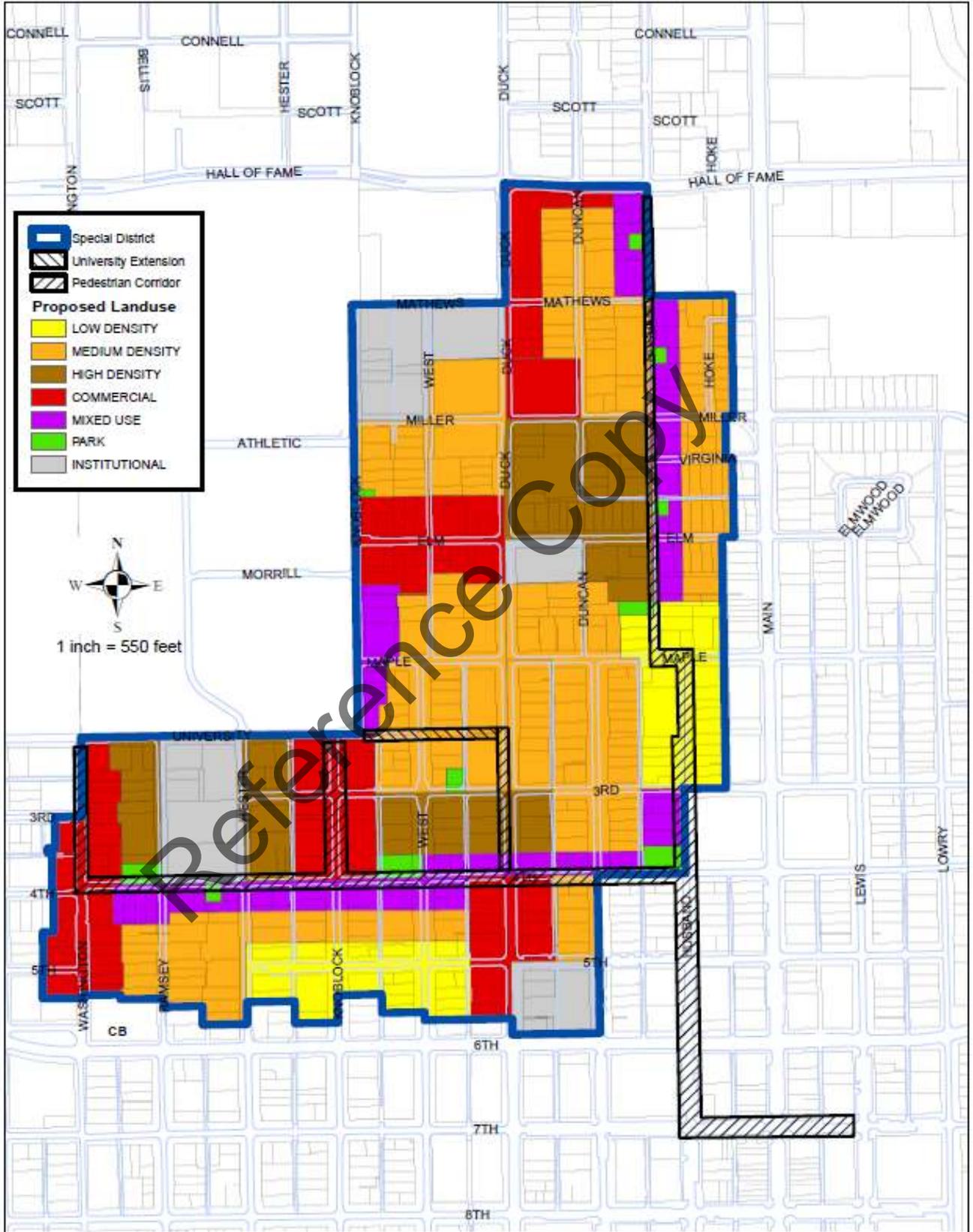
The future land use map identifies each land use classification as envisioned for the Corridor Redevelopment Plan area. The location and extent of each future use is generalized to corridors and complimentary areas, not to individual properties. The map is intended to illustrate relationships among uses in creating an efficient, compatible and viable development pattern for the Corridor Redevelopment Plan area. The map will also serve as a guide in zoning decision making to help implement the future land use map by further identifying specific uses.

This future land use map delineates areas into different categories such as low density residential, medium density residential, high density residential, commercial, mixed use, park, and institutional. Only uses that are determined to fall within these broad designations are allowed within the areas shown on the map. The map also designates where such clusters or areas of allowable development types should occur over the set future timeline of the community.



Typical Redevelopment Plan

Reference Copy



DEVELOPMENT OPPORTUNITIES:

This plan is focused on providing the necessary infrastructure for redevelopment and rehabilitation, on encouraging private investment, on utilizing vacant or underutilized buildings and parcels, and on creating connections between the University and the core commercial districts. Other opportunities will be triggered from the redevelopment and rehabilitation efforts that are taken over the next few years in this area, some of which may be discussed herein.

Improvements, including public and private investments, are necessary to spur the attainment of this plan. Although this plan will not set aside funds to reduce the private interest and investment costs, a companion document will be considered by the City Council establishing an incentive program for the Corridor Redevelopment Plan Area.

The Corridor Redevelopment Plan will serve as a guideline for the area in the realm of future development and decision making. In order to envision and create a vibrant, active, complimentary area, certain uses and activities are being identified for implementation. A certain look and feel is expected throughout the Redevelopment Plan area with eclectic styles, vibrant colors, appropriate lighting and landscaping, and a mixture of old and new in an easy and comfortable feel. Such considerations are not meant to be specific requirements nor rigidly interpreted but are to serve as a guide for investors, decision makers and the general public.

Development Intent:

- ❖ Encourage people-watching
- ❖ Create year-round use.
- ❖ Provide diverse cultural, entertainment, shopping, and living opportunities.
- ❖ Facilitate programming venues for the community at large.
- ❖ Incorporate a rich landscape.



People Watching



People Watching



Local Music

- ❖ Site buildings to define outdoor rooms.
- ❖ Encourage a mix of cultural, retail, dining, and gallery uses.
- ❖ Encourage mixed-use residential with commercial/office spaces.

RESIDENTIAL.

Housing in the Corridor Redevelopment Plan area should consist of a thorough mix of cost and type, ranging from apartments located above retail shops and in high rise buildings to moderately priced rehabilitated bungalows, and from smaller urban condominiums to elegant row houses. A mix of new and rehabilitated housing will help yield a healthy socio-economic diversity and opportunities for many types, sizes and prices of amenities. A key objective is to foster a climate for investment that provides opportunities for new housing, rehabilitation of structurally sound housing, and redevelopment of housing options within the plan area.

Dense & Urban. Currently, the majority of the area is developed as single family residential or multi-family residential. Although residential is the primary use of the area, it is not being used to its highest and best use. With the proximity to the University, a high demand exists for very dense, very urban student housing and not necessarily so in the historical single family or multi-family structures that are common in Stillwater. Also, rare has been the dense, urban residential development in Stillwater that encompasses more than a few lots joined together. In order to provide such dense and urban residential development, half-blocks need to be the minimum lot sizes; hence, incentives to demo the existing outdated or underutilized structures will be necessary. Such dense and urban opportunities will compliment the existing commercial areas and the University by providing both the modern amenities that urban dwellers are attracted to while not competing with existing retailers and businesses. Specific areas for the dense and urban residential development are identified and new zoning



Single Family



Apartments



Apartments

classifications accommodate such uses.

Semi-Urban. While envisioning dense and urban student housing opportunities adjacent to campus, there also will need to be included semi-urban housing opportunities in the form of lower density multi-family and 3- and 4-plex structures. These can be developed on individual lots or on multiple lots joined together and areas will be identified for such development.

Senior housing. As Stillwater continues to attract residents of all ages, housing for the senior population is also encouraged in this Plan Area. Such housing should provide ADA accessible housing for senior populations where eating, shopping, transportation, and medical facilities can be easily accessed. At the same time, life is occurring around this population group that encourages feeling involved and participating in the community. Primarily condos, loft-style quarters, and open floor-plan housing is recommended, especially in mixed-use style buildings and developments.

MIXED-USE.

While currently there are many properties occupying various uses and degrees of development, it is anticipated that the area will develop with many mixed-use structures having retail or other commercial on the ground floor and office uses and residential on the above floors. In order to create uniform building form and match the existing core commercial areas, mixed-use buildings need designs that address the setbacks from the streets/sidewalks. All public rights-of-way adjacent to these developments are to be enhanced with a pedestrian streetscape that encourages walking from the site to adjacent uses. Site and use specific dedicated parking is to be located on site to the rear of the buildings with access from alleys as available. Any on-street parking will compliment the uses needing such parking yet will continue to be for the public.



Semi-Urban



Single Family



Townhomes



Mixed Use

Any revitalization of ground floor commercial or retail uses will take a concerted effort by the City and private property owners and investors. The City can assist through incentive programs, redevelopment opportunities and improvements. Private property owners can assist with external improvements such as attractive and uniform design, use of quality materials, attention to architectural details such as signage, lighting, overhangs, and public art.

The residential upper floor living will also need assistance from the City, OSU and private property owners. Public transportation is critical with limited on-site parking for residential portions of mixed-use developments. Improvements in pedestrian access to the core commercial areas, to OSU campus, to recreational and employment opportunities will encourage more living in the area and fewer automobiles on the streets. This type of living should attract students, young professionals, empty-nesters, and seniors.

COMMERCIAL/RETAIL

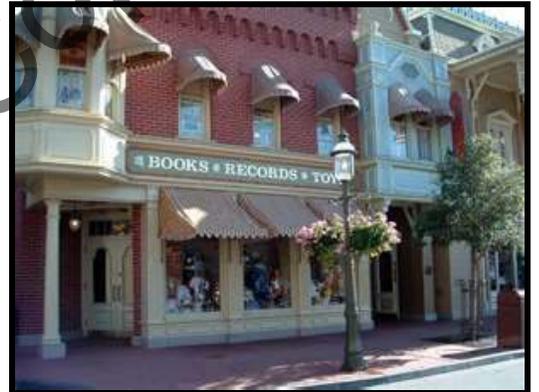
Commercial vitality is integral to the function of a successful urban area. Almost every activity produces some impact on the local economy, therefore objectives for commercial development are as follows.

Density is a priority – conventional, low density, commercial solutions are not appropriate.

Pedestrian Orientation. Building design should assume that the pedestrian will be a primary user of the commercial facility. Reduced parking requirements, use of shared parking facilities, and use of adjacent on-street parking are appropriate. In considering off-street parking, pedestrian orientation will be preserved by placing off-street parking areas behind the building, aligning building fronts with existing structures at or near the right-of-way, and designing pedestrian-friendly elements into the



Mixed Use



Mixed Use



Rear Parking Court

building façade.

External environmental impacts. Urban life is, on the whole, busier, noisier, and less predictable than life in other areas; this is an attractive feature for many people. Commercial activity is less restricted, subsequently the Plan allows a large number of compatible land uses for commercial areas. Uses that are not detailed herein should be reviewed to ensure compatibility with efforts to mitigate the effects of noise, light, and pollution.

Design is important with highly-visible commercial corridors having minimum standards so that infill development and rehabilitation structures will be consistent with the surroundings.

Uses. In any commercial area, a variety of uses are encouraged. In keeping with the goals of this Corridor Redevelopment Plan and the underlying zoning classifications, the following uses are encouraged: grocery, specialty item shops, pharmacy, eye/teeth/light medical, trendy restaurants, trendy boutiques, coffee shops, hair salons, partnership between campus and restaurants for meal plans, outdoor eating areas, gym/fitness facility, outdoor performance area, indoor theater/entertainment, playgrounds, outdoor film area, farmer's market, delivery/personal shopper service, international market, community event area, street vendors, arts, cultural opportunities, bed and breakfast, convention center and hotel.

PEDESTRIAN CORRIDOR

As with any vibrant neighborhood, the ability to have and encourage pedestrian circulation is vital to its success. Passers-by meander, stop, visit, window shop, purchase, rest, and attract others to do the same. With the proposal to provide an open-air pedestrian corridor, many aspects of the corridor are to be further considered and encouraged.



Store Front



Restaurant with Outdoor Seating



Store Front with Outdoor Seating

- As redevelopment or individual owner/city initiative occurs, the pedestrian corridor shall be provided with minimum sidewalk widths of 8-feet on each side of the pedestrian corridor designated streets.
- Pedestrian corridor shall include landscaped areas totaling a minimum of 10% of the entire walkway within each block.
- Trees shall be included at a minimum of one tree per every thirty-five linear feet of corridor length. Trees shall be planted in wells a minimum of forty square feet in area or in planters or in structural soil with pervious pavers, shall be small to medium trees and of species to form a canopy over the pedestrian corridor at maturity.
- Pedestrian amenities such as benches, tables, chairs, flower urns or pots, bike racks, and trash receptacles will be included in the pedestrian corridor.

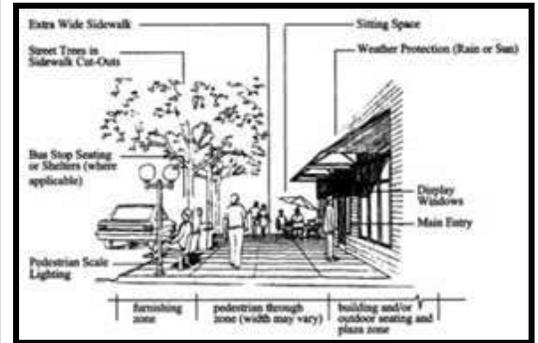
DEVELOPMENT GUIDELINES:

The following guidelines are intended to guide design professionals, property owners, City officials, University officials, and developers in creating new site-specific contextual buildings and areas that incorporate a mixture of uses with pedestrian friendly activity.

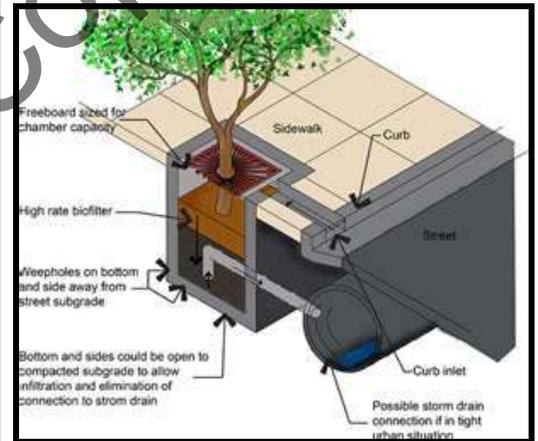
BUILDING ORIENTATION AND SITING

Appropriately sited buildings can greatly enhance the formation of the public streetscape. To the greatest extent possible, buildings should be sited to provide high quality, functional and livable outdoor public spaces that enhance the use of the building, the street frontage, and the surrounding activities.

- Building frontages are encouraged to be located close to the street right-of-way.
- Building configurations that create awkward relationships with adjacent properties, buildings, and streetscape are prohibited.
- Buildings fronting multiple streets shall



Pedestrian Corridor Design



Tree Well



Amenities

have consistent façade treatments on each frontage with respect to materials, scale, proportion and detailing.

- Buildings on corner lots shall incorporate a form that addresses both streets.
- Buildings shall orient facades and main entrances toward the street, toward a plaza, or toward a pedestrian way that directly leads to a street.
- Buildings shall be sited to create active outdoor spaces.
- Service areas, mechanical equipment, and refuse storage areas shall not occur on or along a primary façade. Such areas are to be located to the rear or side of buildings and screened from view from the street and/or public space. The use of alleys is encouraged to locate all mechanical, electrical, and utility equipment to the extent possible.
- The primary entrance to residences and first floor commercial spaces shall be oriented to the street.
- Off-street parking shall be located to the rear of the buildings and shall be screened from view from adjacent streets and commercial properties. Parking between the building and the adjacent street right-of-way is prohibited.
- Residential entries facing a public street shall have a transition element such as a stoop, a small porch, or other feature to differentiate the private entrance from the public sidewalk or commercial entrance.
- Narrow entry gardens between the primary façade and the property line are encouraged in primarily residential areas.

BUILDING FACADES

Forms and elevations of buildings should be detailed and articulated to create interesting facades.

- A mix of complimentary building materials is encouraged for a consistent, systematic treatment of all exterior facades.



Mixed use with a strong corner detail.



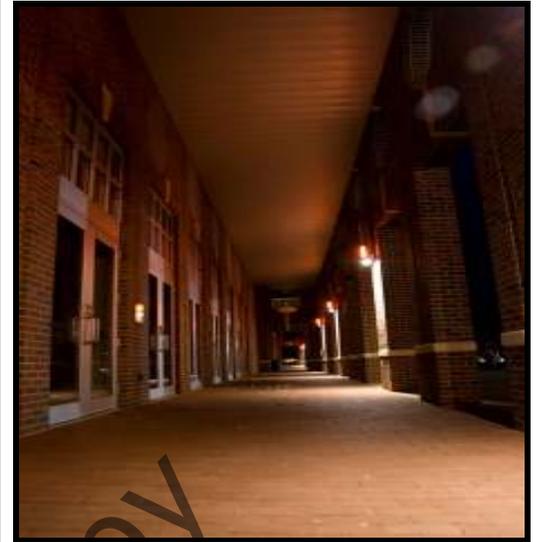
Corner orientation



Residential rear parking

Individual façade elements shall respect the scale of immediately adjacent building elements.

- Vary setbacks on wall surfaces to form entrances, express structural elements, or to create special exterior areas such as planters, seating, alcoves, etc.
- Encouraged are building elements that create strong patterns of shade and shadow.
- Visible rear and side facades shall provide consistent visual interest by incorporating characteristics similar to the front façade.
- All buildings shall be designed specifically for the context and character of the project. Iconic, corporate, or standard building design will be allowed only if they meet the purposes, intent and conditions of these guidelines.
- Provide a concentration of building details at ground or sidewalk level to create visual interest and enhance the pedestrian environment.
- Art and/or specifically designed ornamentation integrated into building facades are encouraged. Art and murals shall not be used as signage or advertising and shall harmonize with the prevailing color palette of the area.
- Large areas of undifferentiated or blank facades are prohibited.
- Each building façade oriented to a street or public space shall provide architectural variety and human scale by use of surface variation at openings, changes in color, texture or materials, art, variation in plane and shadow, recessed entries, projecting sills or balconies.
- Buildings that occupy a corner at two streets or a continual street shall include features that reinforce both street environments including pilasters, corner entry features, tower features and display windows.
- Awnings and canopies are encouraged at entries and/or display windows.



Colonnade



Building art



Building ornamentation

BUILDING PROPORTION AND SCALE

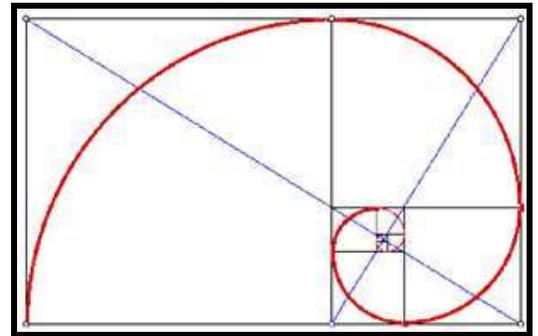
Proportion is the relation between respective parts or between parts and the whole in a building or any work or art. Scale relates to the relationship of one element to another or of one element to the whole.

- Buildings shall be compatible in scale and proportion to buildings within their immediate context and should incorporate architectural elements that relate to the human scale.
- Incorporate elements that give the building perceptible scale. Large buildings in particular shall be designed to reduce their perceived height by dividing the building mass into smaller scale components.
- Rhythm and repetition of building elements is strongly encouraged. This is accomplished through incorporation of regular or patterned units that are organized to provide a continuance, flow or sense of movement.
- Spaces between building masses (alleys, recessed entries, courtyards) shall be scaled and proportioned to maximize the comfort of users. These areas should not be dark or secluded spaces and shall incorporate detailing that adds interest, orientation, and spatial definition.
- Use columns, fenestration, doorways, roof elements, wall patterns, light fixtures, signs, paving patterns and landscape to create rhythm.

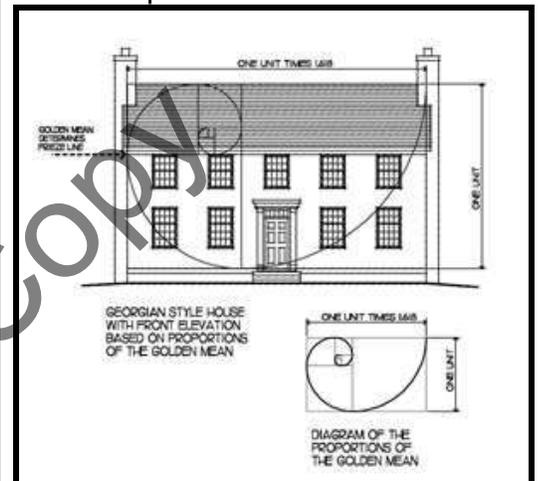
BUILDING ENTRANCES

Entrances are to be designed to allow individual businesses and properties to present a clear defining image without compromising the sense of unity of the whole building block or façade.

- Entrances shall be accentuated and oriented in a way the engages the public street.



Golden Spiral



Scale of building using the Golden Spiral.



Example of proportion.



Scale of structure to pedestrians.

- Entrances shall be easily identifiable.
- Entrances to individual stores or uses shall be articulated.
- Large buildings that front multiple streets are encouraged to provide multiple entrances. Likewise, buildings that front on-site parking areas shall provide entrances convenient to the parking areas.
- Recessed entries provide a pleasant transition from the sidewalk to the interior of a building and helps ease the change from public street to the more private interior. Recessed entrances also provide a safe place for entry doors to open without extending into the public right-of-way or sidewalk.

DISPLAY WINDOWS

Storefronts offer the greatest opportunity to attract the attention of the passer-by.

- Historic features such as decorative moldings are to be preserved with new windows matching the original.
- When restoring an existing building, tinted or textured glass is to be replaced with transparent glass to increase visibility.
- Glass block shall not be used for storefront display windows.
- Avoid horizontal window division and framing.
- Elevate display windows above sidewalks approximately 12 to 18 inches.

BUILDING SIGNAGE

Building signs convey more about an individual business and the area than any other storefront element. Signs can add visual interest and detail to a building as well as communicate the quality of the business within the building and within the area. To be effective, signs must be harmonious in scale and color with the subject building and neighboring buildings.

- Signs shall fit the overall proportions of



Recessed entrance



Store entrance and entrance to residential units.



Recessed store front with large display windows.

the building and be integrated into the building design.

- Signs shall be mounted no higher than the bottom of the second story windows in multi-story buildings.
- Signs shall not call attention to themselves at the expense of neighboring buildings by virtue of their color, scale, lighting, materials, or other obtrusive features. Signs shall not dominate the building façade.
- Signs that impact surrounding residential areas with light pollution or obstruction of sight lines are prohibited.
- Secondary and/or rear entrance signs are required when parking is available behind or on the sides of the building or business, or where deliveries are made.
- Advertising placards which obscure views into and/or out of windows are prohibited in storefront windows.
- Signs shall not interfere with operable doors, windows or architectural detailing.
- “Shingles” or projecting signs that are pedestrian oriented are encouraged and shall be designed to be proportional to the building façade.
- Directory signs for buildings containing multiple businesses, uses or addresses are permitted.
- Creative signs incorporated into the building are encouraged.

PEDESTRIAN ENVIRONMENT

Convenient, safe, and direct pedestrian access to individual properties and to associated streets and activities is critical to the success of the area. New development and redevelopment should be designed for the pedestrian as well as the vehicle.

- Primary sidewalks are those which are located along a street frontage; secondary sidewalks are those which are internal to individual parcels and serve parking and or service areas.



Integrated sign on building.



Signs with gooseneck lighting.



Large primary sidewalk

- Primary sidewalks shall be no less than 6-foot wide and locate on both sides of the street. Secondary sidewalks shall be no less than 5-foot wide and directly connect the individual parcel, parking, or service area with a primary sidewalk.
- Sidewalks that incorporate activities such as dining, sitting areas, open entertainment venues, or similar activity shall be a minimum of 10-foot wide and shall provide an open pedestrian corridor to promote safe pedestrian flow and unrestricted access.
- Amenities such as planters, urns, benches, bike racks, and trash receptacles required for development of properties shall be coordinated with the City to ensure compatibility with public streetscape improvements. Amenities shall be placed as to provide a continuous clear zone for pedestrians and to avoid conflict with vehicular sight lines at ingress/egress locations.
- Newspaper and magazine types publication boxes shall be grouped together and placed within a screened element to minimize the visual impact along the corridor.
- Service and delivery access areas shall be separated from on-street parking areas and sidewalks and shall be discreetly located by utilizing alley access whenever possible.

SCREENING

The overall visual quality of the Corridor Redevelopment Plan area can be improved by minimizing or eliminating the views to undesirable areas. Appropriate screening of parking lots, delivery areas, loading docks, dumpsters, ground mounted mechanical equipment, utility service connections, ice and vending machines, freezers and coolers, and transformers can greatly enhance the feel and look of an area.



Primary sidewalk



Benches



Parking lot screening



Utility screening

- Where service areas and utility banks are located adjacent to residential property, appropriate year-round live screening shall be maintained.
- Screening can be accomplished by the incorporation of softscape or hardscape.
- Plant materials are encouraged for screening uses if they provide sufficient year-round density to screen adequately.
- Architectural elements such as fences and walls shall be of solid construction to prevent visibility of the area being screened. The use of fences and walls to screen equipment or service areas shall be located and sized to conceal the subject equipment and area.
- Fences and walls are to be designed to reflect and/or compliment the architectural style of the adjacent building and shall incorporate similar materials to that of the adjacent building.
- Screening elements are to be maintained in good condition and repair. Painted surfaces are to be free of peeling and faded paint. Landscape screening is to be trimmed to provide a quality appearance and shall incorporate underground irrigation for watering purposes.

LANDSCAPING

Effective and attractive landscaping can greatly enhance property values and contribute to the year-round pedestrian experience throughout the area.

- Property owners shall provide landscaping on private property that is compatible with that in the area to ensure a consistent appearance.
- Shade along the pedestrian corridor is a priority. Street tree plantings utilizing fruitless species are encouraged.
- Automatic irrigation systems are required for landscaped areas and street tree plantings. When an area is too small to warrant an automatic irrigation system,



Service area screening



Low impact development streetside rain garden

the incorporation of “quick couplers” are encouraged to provide the water service necessary for manual watering.

- Regular maintenance of plant materials such as watering, fertilizing and pruning will provide a quality appearance.

PARKS

Multi-use parks are a major component of revitalization and redevelopment for an area. Parks can function as outdoor gathering places, entertainment venues, play areas for children and families, flood control areas, sporting event venues, relaxed recreational venues, and habitats for birds and other animals that survive in urban environments. In order to encourage parks as part of the redevelopment area, certain areas will be identified on the Plan Map to incorporate an urban park or pocket park into development and redevelopment plans.

SITE LIGHTING

Effective site lighting can enhance the pedestrian and vehicular environments and can encourage nighttime activities.

- Lighting shall be architecturally integrated with the overall site design, pedestrian and vehicular circulation routes.
- Control of light intensity to limit light pollution and glare in residential areas is required. Except for very low wattage ornamental lighting, all fixtures must be full cut-off design mounted in a horizontal position.
- Use down-lighting, bollards or wall-mounted sconces to reinforce circulation corridors.
- Area lighting shall result in an average minimum 0.5 foot candle at all pedestrian areas.
- Flashing, pulsating, or moving lights and non-commercial lighting fixtures are prohibited.
- The use of color correct fixtures such as metal halide is encouraged.



Amphitheater



Artistic lighting



Bollard lighting

- Light sources are to be directed away from residential neighborhoods with full cut-off hoods and shades.
- At pedestrian promenades and primary sidewalks, ornamental light fixtures shall be used to enhance the pedestrian environment and to relate new development areas to the rest of the Plan Area. The light fixture style, material, color and scale shall be coordinated with the public right-of-way lighting for the area.
- At public plazas, cable suspended light fixtures may be used to differentiate the space from the rest of the pedestrian and vehicle circulation paths and to promote a festive atmosphere.

PARKING

Parking is always a requirement of any development but is rarely utilized as a positive aesthetic tool of development. Parking quantity should not be the only issue considered in design. Physical organization, distribution, pedestrian links, and screening are issues that must be addressed in parking lots and with on-street parking.

- Parking lot configurations shall conform to City standards unless otherwise approved.
- Parking lot islands and green space shall be incorporated within all parking lots to provide shade and visual relief from large expanses of pavement. Parking lots shall be provided with a minimum of 6% green space.
- Parking lots shall be effectively screened from the surrounding street network. Where a parking lot abuts a street, a minimum 8-foot side buffer, including sidewalk, shall be installed to effectively screen views to parked vehicles. Where a parking lot abuts an interior property line, a minimum of 4-feet shall be maintained as green space. Screening for parking shall be a minimum height of 3 feet and



Pedestrian scale lighting



Shaded parking lot



Screened parking lot

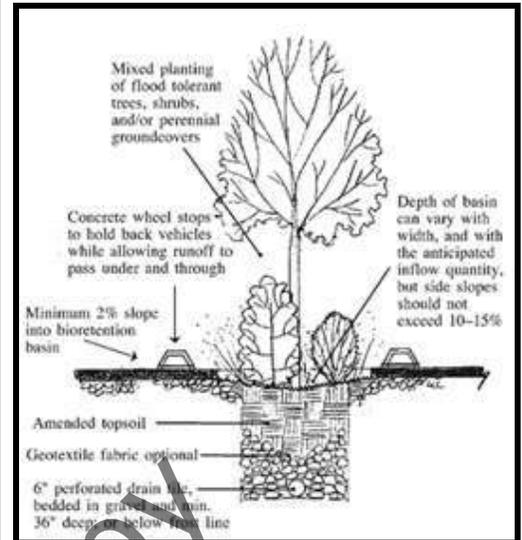
be of either softscape (vegetation), hardscape (walls or fencing), or a combination of both.

- Parking lots and on-street parking design should provide a clear, direct path between the parking areas and the business destination.
- Generally, parking lots are to be located at the side or rear of buildings. Parking lots shall not be located between a primary building façade and the street unless no alternative exists.
- Parking lot pavement materials shall be reinforced concrete, asphalt, or permeable alternative. Dirt and/or gravel surfaces are prohibited.
- Residential off-street parking shall be located to the rear of the buildings and shall be screened from view from adjacent streets and commercial properties.
- Private residential garages are encouraged and entrances shall not open on a public street or open space but onto a parking lot or alley.

PARKING STRUCTURES

The Corridor Redevelopment Plan area will include at least one shared-use parking structure intended to be located and designed to provide convenient access from high-volume activities such as theaters, dining, sporting events and residential living.

- To the greatest extent possible, parking structures should be “embedded” or placed to the back of buildings.
- The inclusion of retail, commercial or office uses is encouraged along street frontages at the ground floor of parking structures.
- The primary entrances to parking structures should be visible and easily accessed from the primary pedestrian and vehicular routes.
- Mechanical equipment and refuse containers should be located at the back



Low impact development parking lot island



Parking Meter technology advances



Parking garage

of parking structures and shall be screened from view from public streets and adjacent properties.

- Stairs serving the levels of the parking structure should be located within view of public streets and pedestrian ways and should be highly transparent for passive security.
- Parking structure facades which face on a street or public open space should have features designed to be compatible with neighboring buildings.
- The use of plant material as a screening element where facades face streets or residential properties is encouraged. Open grillwork or trellises supporting climbing plants can make an effective and attractive screen, create a green wall and maintain ventilation.

Wayfinding and Signage.

Wayfinding is the orderly structuring of information required to enable people to comfortably and successfully access an area or specific destination. Throughout the entire Plan Area, wayfinding is lacking and the wayfinding that is available is in need of updating. Following are objectives and suggestions for incorporating wayfinding and signage in the Plan Area.

- Increase the number of people who visit/use the Corridor Redevelopment Plan area.
- Identify the boundaries of the Plan Area.
- Announce to visitors their arrival in the Plan Area.
- Reflect the area's structure and vision.
- Enhance the area's public image through distinctive, helpful graphics.
- Make visitors' and citizens' experiences more memorable.
- Improve vehicular and pedestrian safety by providing better information.
- Guide people into, through and out of the area and to businesses, attractions, parks, historic buildings, cultural venues.



Parking garage with bottom level retail.



District identification



Wayfinding signage

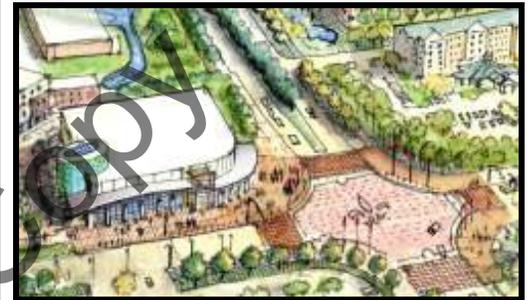
- Reduce frustration of unnecessary circulation resulting from misdirected travel to destinations and parking.
- Define destinations and parking in a more identifiable way.
- Properly orient people as they leave parking areas and become pedestrians.

A key to wayfinding is the consistent presentation of information between sign types, which reinforces the wayfinding logic. On vehicular and pedestrian directional signage, destinations should be grouped by direction and presented in a set order. Vehicular signage should give drivers time to anticipate upcoming turns. With each directional grouping, destinations should be listed alphabetically to help drivers and pedestrians quickly locate their destinations.

Sign types should include Welcome Signs, Corridor Identifier Signs, Vehicular Guide Signs, Parking Identifier Signs, Pedestrian Guide Signs, and Pedestrian map Kiosks.



Types of wayfinding signage

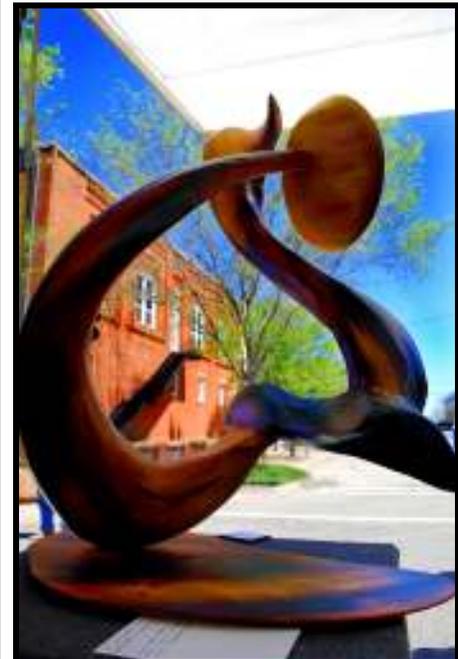


Architecture and paving as wayfinding tools.

COMPLIANCE WITH COMPREHENSIVE PLAN:

This Corridor Redevelopment Plan is consistent with the Stillwater Comprehensive Community Plan adopted in 2001. Applicable goals of the Comprehensive Plan are:

1. To be a community building around a Downtown which functions as a cultural and civic heart in our community;
2. To have a Downtown that serves as a center for high quality business services;
3. To foster our small town atmosphere by maintaining a safe, attractive environment;
4. To maintain the safety that we have always had, being able to walk anywhere in the community and feel safe in our homes, workplace and schools;
5. To foster unsurpassed community friendliness and neighbors who know and care about each other;
6. To be a community that successfully knits and celebrates its diverse entities;



Downtown art

7. To provide better amenities for elderly, youth, and disabled;
8. To build our economy and promote business development;
9. To enhance retail opportunities throughout the community;
10. To build our economy by developing and combining local resources;
11. To be a steward of a visually appealing environment;
12. To have main entries/corridors well-planned and designed;
13. To be stewards of an ecologically sound community;
14. To be a well-planned and attractive community which nurtures economic growth and development while maintaining a quality of life;
15. To provide an open space greenway/park system linking community components such as neighborhoods, Downtown, OSU, outlying lakes, streams, etc.;
16. To connect neighborhoods and the people within them;
17. To provide attractive linkages between the community's diverse uses and people; providing for transit, walking and biking so that all people have access to essential services.

IMPACT OF REDEVELOPMENT:

Without the adoption of the Corridor Redevelopment Plan and any subsequent complimentary incentive program, this area is not reasonably expected to be redeveloped to the level envisioned herein. In the absence of City-sponsored redevelopment and partnership opportunities between the City of Stillwater, Oklahoma State University and private investors, there is a prospect that blighting factors will continue and spread to the adjacent neighborhoods and the area as a whole will become less attractive for the maintenance and improvement of existing buildings and sites. Erosion of the assessed value of property in and around the Corridor Redevelopment Plan Area



Downtown landscaped intersection



Downtown bus stop



Downtown crowd

could lead to a reduction of real estates revenue and sales tax revenue.

Implementation of the Corridor Redevelopment Plan is expected to have significant short and long-term positive impacts. With any improvements to the existing infrastructure and other incentives for redevelopment and reinvestment, private investors will consider this area of the community when looking to develop student and/or dense urban-style living. Such living opportunities will encourage the commercial, retail, and office enterprises that these residents demand. Long-term impacts, after the completion of all redevelopment improvements and activities, is the benefit from any enhanced tax base resulting from the increase in assessed value of properties in the Corridor Redevelopment Plan Area. Also, more and more people will live, work, study, shop, and play in the Corridor Redevelopment Plan Area, continuing to input life and vitality back into the vicinity.

As a result of rehabilitation and redevelopment within the Redevelopment Area, Stillwater and OSU may experience an increased demand for services. The increases may warrant the hiring of additional staff or the need to evaluate how such services are provided. Services having more demands placed upon them include, but are not limited to, police, fire, building inspections, utility upgrades or repairs, sanitation collection, and potential use of parks and other publically operated recreational or entertainment venues.

Increases to the Stillwater Public School district is possible with the redevelopment of this area. As Stillwater continues to grow in population, increases in the number of school-age students is evaluated by the School Board on a regular basis.



Police department redbuds



Multi-use trail

SUMMARY:

Stillwater, Oklahoma. A place where citizens can live, work, and play. In keeping this community a magnet for economic development that attracts people, the Corridor Redevelopment Plan is adopted as a guide for our future. This plan helps us learn how to attract a diverse and complimentary mix of uses, to understand the value of place making in creating a high-quality environment, to see how the image of the community directly relates to the quality of life.

City officials, OSU officials, private land owners and developers are encouraged to use this Corridor Redevelopment Plan as the guidance and principles needed to make this area of Stillwater the most successful and innovative that it can be.



Sheerer Museum

Reference Copy